



**infrastructure**

planning and building control  
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**DEPARTMENT OF INFRASTRUCTURE**

TOWN AND COUNTRY PLANNING ACT 1999

TOWN AND COUNTRY PLANNING (REGISTERED BUILDINGS) REGULATIONS 2013

NOTICE IS HEREBY GIVEN that at a meeting held on the 21st October 2014, the Department of Infrastructure, in pursuance of its powers under the above Acts and Regulations REGISTERED

**Ballagawne Gate House Mount Gawne Road Colby Isle Of Man**

as represented by the buildings delineated on the plan hereto attached, in the PROTECTED BUILDINGS REGISTER by reason of its special architectural and historic interest.

THE EFFECT OF THIS REGISTRATION IS IMMEDIATE and prohibits the alteration or demolition of the structure or appearance of any part of the building except in compliance with an obligation imposed by or under any statutory provision or with the prior written consent of the Planning Authority.

Dated this 2nd October 2013,  
By Order of the Department

A handwritten signature in black ink, appearing to be 'C A Dudley', written over a horizontal line.

**C A Dudley**  
**Deputy Secretary to the Planning Committee.**

1<sup>st</sup> Floor, Murray House,  
Mount Havelock,  
Douglas,  
IM1 2SF.

NOTE :

***Under The Town and Country Planning Act 1999;***

**Schedule 2  
The Protected Buildings Register**

*Notifications of entries on register etc.*

2. (1) As soon as may be after a building has been entered in the register, or the register has been amended by removal of a building from it, the Department shall serve a notice on the owner and the occupier of the building stating that it has been entered in or removed from the register.
- (2) The owner or the occupier of, and any other person having an interest in, a building which has been entered in the register may apply to the Department to remove the building from the register-
- (a) within the prescribed period after service on him of a notice under sub-paragraph (1);
- (b) after the expiration of the prescribed period after the decision of the Department on a previous request under subsection in relation to the building.

***Under the Town and Country Planning (Registered Buildings) Regulations 2013***

**5. Periods for purpose of Schedule 2 paragraph 2(2)**

- (1) The period specified for the purposes of paragraph 2(2)(a) of Schedule 2 to the Act (period after notice of registration, within which owner or occupier may request de-registration) is 21 days.
- (2) The period specified for the purposes of paragraph 2(2)(b) of Schedule 2 to the Act (period after initial period, during which owner or occupier may not request a de-registration) is 5 years.

To: **Property Owner – FILE COPY**

Local Authority - RUSHEN, Newlands , Ballagawne Road, Colby, IM9 4AX  
Director, Manx National Heritage  
Isle Of Man Natural History & Antiquarian Society

# REGISTERED BUILDING PROPOSAL

**NAME:** Ballagawne Gatehouse  
**ADDRESS:** Mount Gawne Road, Port St. Mary  
**POST CODE:** IM9 5LX  
**LOCAL AUTHORITY:** Rushen Parish Commissioners  
**GRID REF:** 215 693

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## Introduction

The Douglas to Port Erin railway opened to the public on the 1<sup>st</sup> August 1874, one year after the Douglas to Peel line opened.

Towards the end of the construction period, stringent economies were necessary to allow completion of the line. This was particularly evident in the station buildings constructed at the time, but it is also apparent in the gate keeper's accommodation on the level crossings. The Peel line level crossings had stone lodges built from the start, whereas on the Port Erin line small wooden huts had to suffice. Several of these were later rebuilt in the form of the stone gatehouses we see on the Port Erin line today.

The role of the gate keeper is to protect the crossing; on the approach of the train he opens the gates for the train and thus closes them to road users. A flag is then shown to the locomotive driver to indicate it's safe to proceed. After the passage of the train the gates are opened for road users.

## History

Today's surviving gatehouses at Ballalona, Ballasalla, Mill Road, The Level, Ballagawne and Four Roads provide a good cross sectional example of the different styles of Gatehouses once seen throughout the Isle of Man Railway Co.'s lines. Ballalona (Santon) and Mill Road (Castletown) are good examples of the earliest form of accommodation i.e. timber construction with corrugated iron roof, although the layout of both these huts differs. Mill Road is believed to be the original hut at this location and dates from the line's opening in 1874.

The stone gatehouses at The Level (Colby) and Four Roads (Port St.Mary) are almost identical and relatively unaltered since built. Four similar structures also still exist on the now closed line to Peel; Quarterbridge, Closemoore, Crosby and Ballacraine. However these four are near derelict, although Closemoore, Crosby and Ballacraine do still provide a very basic form of shelter on the 'Steam Heritage Trail'.

The stone gatehouse at Ballasalla is similar to the ones at The Level and Four Roads, however an extension was provided in 1962 by the Isle of Man Railway Co. to house a 'ship's wheel' with which to operate the mechanical gates and observe the road.

The final example at Ballagawne (Port St.Mary) is again of stone construction, but was extended by the Isle of Man Railway Company many years ago to provide living accommodation for the keeper and his family.

During 2003 the Castletown to Port St.Mary section of the Douglas to Port Erin line was re-laid as part of IRIS, the Island Sewage Project. Automation of the road crossings was part of this project and Ballagawne was so treated with the electrical control cabinets being mounted on the west wall. Ballagawne had ceased to provide living accommodation for the crossing keeper many years previous and the building was in a poor condition prior to automation. However since automation there has been no need to provide even basic shelter for a crossing keeper and thus the building has been allowed to fall into a very bad state of repair. During a 2013 survey access to the extension areas was prevented due to the poor state of the wooden floor and acro props holding-up the roof members. The original stone structure appeared relatively sound, however inspection of the roof space was prevented due to a false ceiling.

As of the 2013 season only Ballastrang Crossing has manual gates and is permanently manned; its staff member being provided with a basic modern hut. A bond currently exists on this crossing preventing it from being automated; it is believed that steps are being taken to try and lift this bond. When this bond is eventually removed and the crossing automated, another aspect of the Isle of Man Railway will pass into history forever.

## **RECOMMENDATION**

Ballagawne Gatehouse is suggested for inclusion on the Protected Buildings Register for the following reasons;

### Age and Rarity

- Of the surviving gatehouses Ballagawne is the only example of one extended by the Isle of Man Railway Company in order to provide living accommodation for the keeper. However due to the extremely poor condition of the extension a considerable amount of rebuilding work will be necessary before the building can be used again. Any development of this building / site should be in keeping with the original structure and the 'Heritage Railway Environment'.
- Ballagawne is a heritage landmark on today's Steam Railway.

### Architectural Interest and/or Aesthetic Quality.

- The original structure is stone built measuring 15'x10' in plan, 8' tall at eaves and 10'-6" at ridge, complete with red brick edging at the windows, door and lintels, the roof is grey slate. The layout drawing on sheet 3 helps illustrate the original stone structure and how the Isle of Man Railway Company extended the property to the north and east; these extensions were rendered and the whole building white washed.

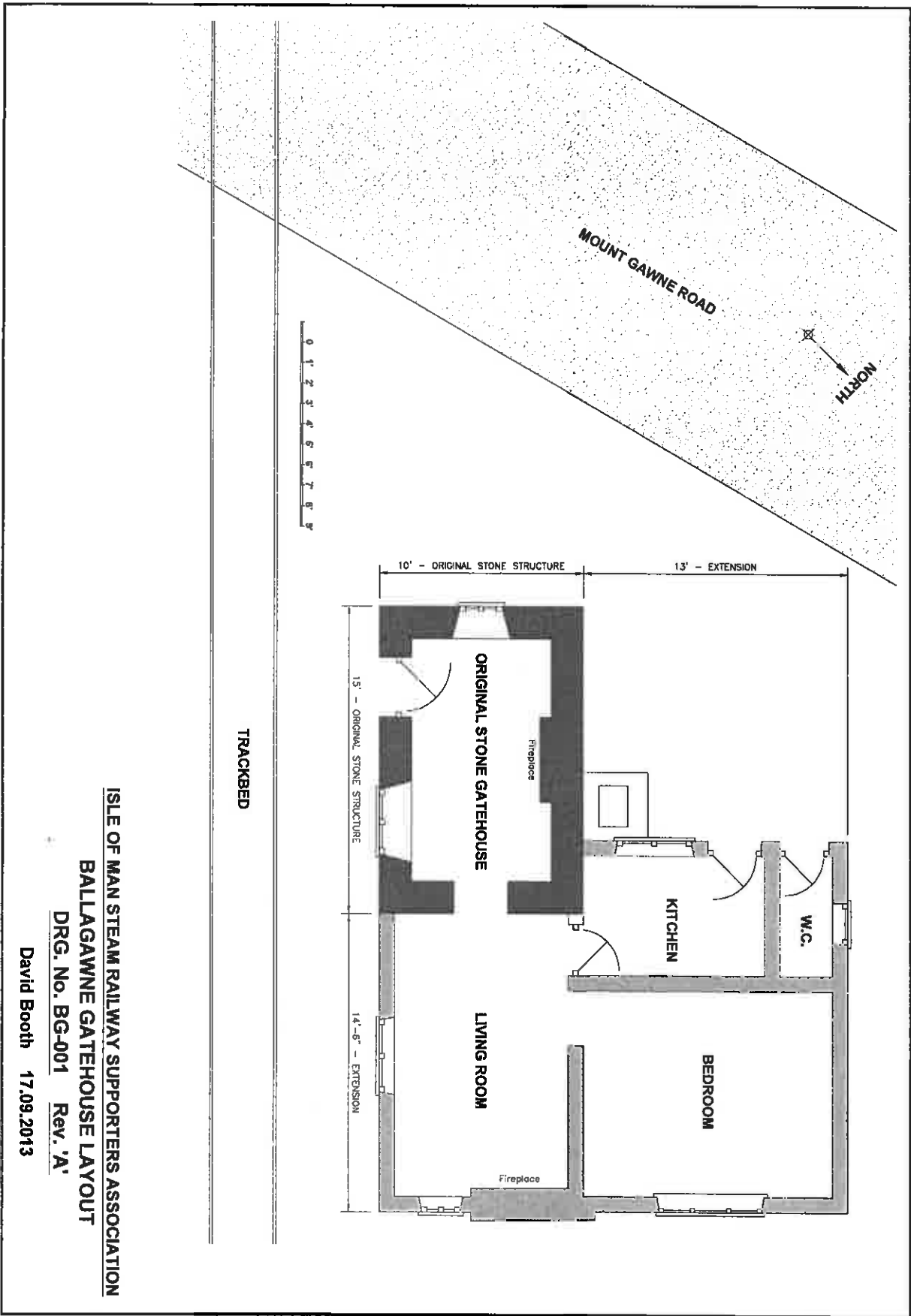
### Historic Interest

- Once Ballastrang is automated, the gatehouse at Ballagawne will become a permanent reminder of how the railway used to operate i.e. a member of staff resident in the gatehouse with his family, spending day after day looking after and guarding the crossing. The social history side of the railway should not be forgotten; this building's former role helps prevent that.

## **SOURCES AND REFERENCES**

### Books and Documents

**James I C Boyd**, The Isle of Man Railway, *Oakwood Press 1962 (revised 1993)*.



ISLE OF MAN STEAM RAILWAY SUPPORTERS ASSOCIATION  
 BALLAGAWNE GATEHOUSE LAYOUT  
 DRG. No. BG-001 Rev. 'A'

David Booth 17.09.2013



This August 2013 view illustrates the original stone built gatehouse in the foreground with the rendered extension to the right.



This 2013 view illustrates the rendered extension at the front and rear of the building.

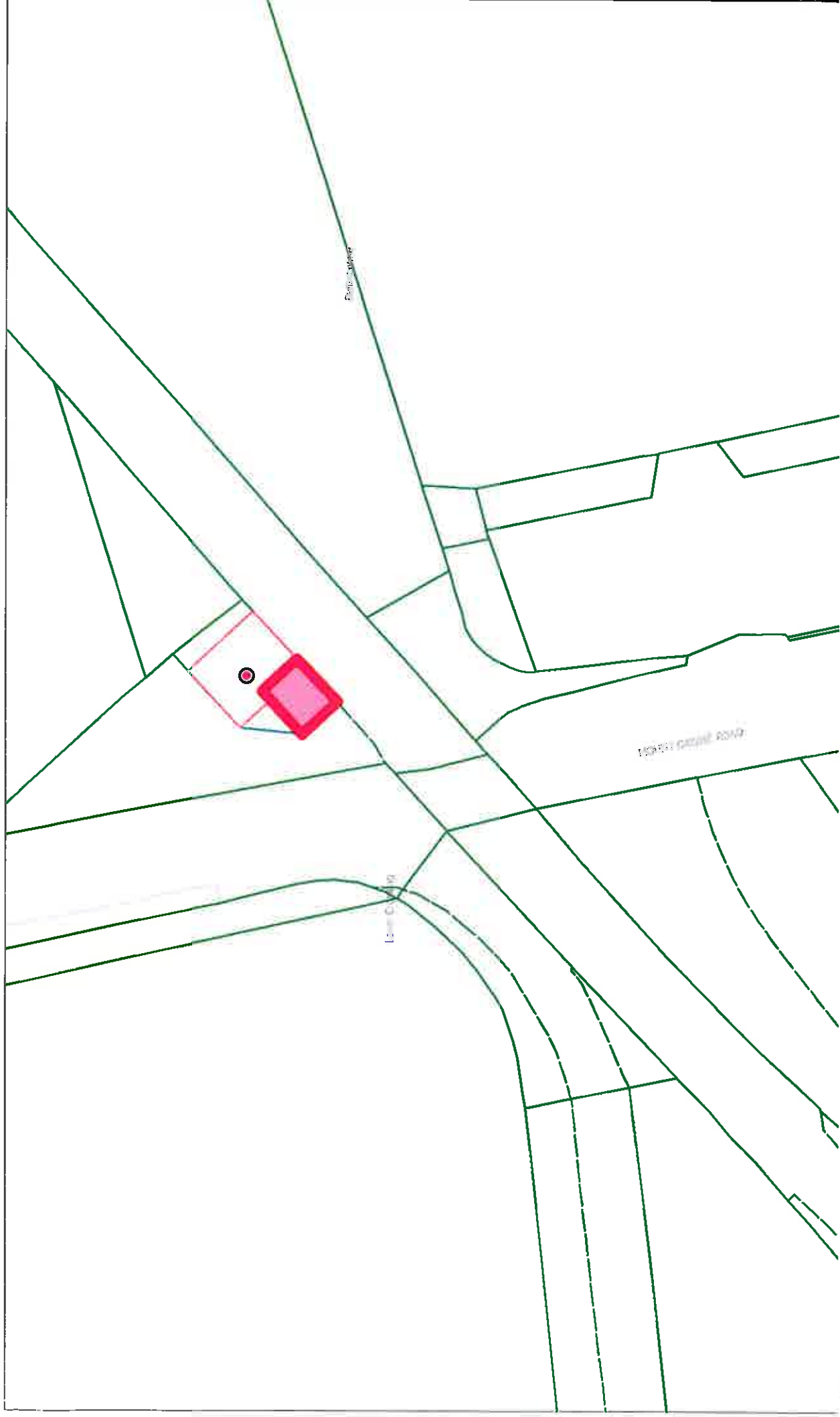


# Department of Infrastructure

Isle of Man Planning and Building Control Division



Date	21 October 2014
Comments	13/00280/REGBLD



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