

to the premises of Ballaconley Engineering, and the concrete interlocking tiled roof to Windmill Terrace.

- 18.4 The shiplap-clad property, with uPVC door and windows, situated between Ash House and Laburnum Cottage, is wholly inappropriate. A similar criticism is levelled at the current telephone exchange adjacent to the Methodist chapel.
- 18.5 Ash House is in a semi-derelict state, and has been so for a number of years. It is partially boarded-up and has obviously suffered substantial settlement damage. It would be beneficial to the area if it were either renovated, or removed and replaced.
- 18.6 The garage and filling station premises within the conservation area do nothing to enhance the character, but severely detract from it. The Total Filling Station is a typical twenty-first century filling-station-clone, designed with little thought for the environment in which it is situated. It has a modern canopy and totem pole-style sign, both of these and the signs on the shop are bright red. The main building is a mixture of brick and block that is painted externally, and has a pitched roof clad in asbestos sheets.
- 18.7 The car sales' site of the Michael Car Centre also detracts severely from the conservation area. This has been created by the removal of the former police house (The Gables), and the car showroom that was fire damaged.
- 18.8 The result has left an unsurfaced, open car sales' pitch, crowded with vehicles, and temporary timber buildings to the rear of the site.
- 18.9 On such an important site, this is a severe negative factor and any steps that can be taken to improve upon the situation should be taken.
- 18.10 At the entrance to Kelly's Yard, there is a pre-cast concrete detached garage that appears to belong to one of the schoolhouse cottages. This is in a highly visible

position, and its eventual replacement with a more appropriate structure should be encouraged.

- 18.11 A communications mast is visible on the hillside to the east of the main road. This does nothing to enhance views out of the conservation area, and the benefits such structures afford must be weighed against the visual intrusion.
- 18.12 At the northern end of the village are situated the Pennybridge Stables. In recent years an indoor riding school of steel portal frame construction, clad in corrugated panels with a matching corrugated roof, has been built. Unfortunately when entering the village from the north, the blank, flat rear elevation of this building is particularly dominant and unsightly.
- 18.13 The gable wall of this building is also visible adjacent to the Total Filling Station. This building is out of scale and proportion with anything else within its vicinity, and dominates the vista to the west of the road when entering the village. There is, however, a triangular area of coppice directly in front of the rear wall of the building, and the impact should be softened in future years as these trees mature.

19. **NEUTRAL AREAS**

- 19.1 Kelly's Yard, the former workshops of Kelly Brothers Limited, Joiners and Builders, is barely visible from the main road through Kirk Michael. It is largely shielded by Windmill Terrace, which stands in front of the majority of the buildings. This is fortunate, as the buildings are an untidy jumble of differing architectural styles, none of which are considered to have any merit.
- 19.2 If Windmill Terrace were not blocking the view of these buildings, their appearance would be considered negative. However, whilst they are capable of enhancement, it is not considered that this would be particularly beneficial to the conservation area due to the limited impact they have.
- 19.3 There are a number of individual properties that have been built in more recent years, which have been included in the conservation area and could be considered to have a neutral status. Whilst these neither enhance nor detract from the character of the area, they are by necessity included to prevent unnecessary deviations of the boundary.
- 19.4 The premises of Kirk Michael Motors, on the corner of Station Road, fits reasonably well into the area and is considered to be neutral. However, the sign on the gable wall is garish and there is scope for improvement.

20. **SUBJECTS FOR ENHANCEMENT**

- 20.1 Whilst conservation areas are designated to preserve character which already exists, they have a dual purpose in identifying subjects where enhancement is possible.
- 20.2 Whilst many negative factors have been identified, it is clearly not feasible in all of these cases for enhancement works to be identified and encouraged within the conservation framework. This is probably the case with regard to the major negative factor within the conservation area, this being the traffic problem.
- 20.3 There is no doubt that the designated area would be substantially enhanced if this were to be addressed, and whilst the issue can be identified within this study: its solution rests elsewhere.
- 20.4 Linked to the traffic problem, is that of off-street parking and is noted within the heading 'Negative Factors'. It would be beneficial to provide further off-street parking to serve the retail units between The Convenience Store and the Post Office, and if this were the case, on-street parking between these points could be prohibited and this would significantly aid the vehicular flow through the village.
- 20.5 Enhancement works do require expenditure, but in some cases this can be relatively modest. A good example of this would be replacing the existing plastic litterbins with units more in keeping with the area. To replace the galvanised steel lamp standards would be more expensive, but these do have a limited life. Consideration for their replacement with standards more in keeping with the area should be given in the future.
- 20.6 As identified under the heading 'Negative Factors', a small number of properties can inflict a considerable negative impact on a conservation area. It is recommended that steps should be taken to address these negative impacts,

either under the Town and Country Planning Legislation, where appropriate, or by negotiation with the owners.

- 20.7 Examples of this would be to replace inappropriate roofing materials, such as the asbestos sheet roof to Ballaconley Engineering, and the concrete interlocking tiled roof to Windmill Terrace with slate. Similarly, in the future, where replacement windows are to be installed, encouragement should be given to using window styles appropriate to the property in which they are to be fitted.
- 20.8 Park View Methodist Chapel is a handsome stone building, the windows of which have been, unfortunately, covered. On the front elevation, these are covered with a green-tinted perspex, whilst on the gable wall the windows are boarded over with timber. Both of these treatments serve to detract from the otherwise attractive appearance of this landmark building, and encouragement should be given to return the appearance of the windows to their original state.
- 20.9 It is most unfortunate that whilst the majority of Windmill Terrace remains in natural stone, one property has been rendered. Encouragement should be given to remove the cement-render coat to create a more uniform appearance.
- 20.10 A metal barrier surrounds the main car park to the village that is situated to the rear of the Isle of Man Bank. Whilst this is functional, it is also unsightly.
- 20.11 This visual negative factor could easily be removed by the building of a low stone wall on the exterior of the barrier, which would shield it from view from outside the car park.
- 20.12 here are a number of overhead cables within the conservation area, and where ever possible, in the future, encouragement should be given for these to be placed underground.

20.13 Within the designated area, no advertisements were noted that were considered to be positive. Two advertisements in particular are considered to be negative and capable of enhancement. These are: the boarded-over window to part of The Convenience Store, which should ideally be removed and the shop front reinstated; and the sign on the gable wall of Kirk Michael Motors.

20.14 The clubhouse of Kirk Michael Football Club is a modern structure with bland elevations. From the football ground the building harmonises reasonably well with its surroundings. However, from Glen Balleira its featureless elevations are dominant due to its elevated position.

21. **CONSERVATION AREA BOUNDARY**

- 21.1 The boundary of the conservation area is outlined on the map. (Appendix 5)
- 21.2 It is necessary to pose the question as to whether the area by the boundary justifies the additional controls which designation brings, or whether it is a matter of applying the normal process of development control that recognises and respects the character and appearance of the conservation area.
- 21.3 The boundary has been kept as regular as possible, but several deviations have been made to allow modern housing developments to be excluded. Wherever possible, field and property boundaries have been utilised, as this is helpful for identification purposes.
- 21.4 As part of the public consultation process, the proposed conservation area boundary was open to suitable amendments given sufficient justification. One such area which was subsequently amended in light of new evidence was the area known as The Fairfield.
- 21.5 The Fairfield site is an area of land adjacent to that of the school. It should be noted however, that such an inclusion does not preclude any future development.

REFERENCES AND ACKNOWLEDGEMENTS

A Michael Village Trail. T L Cashin.

Michael Heritage Trust. 1986 Rev 1999.

Kirk Michael Isle of Man: A Guide to Village & Parish. John Grimsom

Michael Commissioners. 1986 Rev 1991.

St Michael's Church and its cross-slabs Kirk Michael, Isle of Man.

F R Gullick.

We wish to express our appreciation for invaluable assistance given by:

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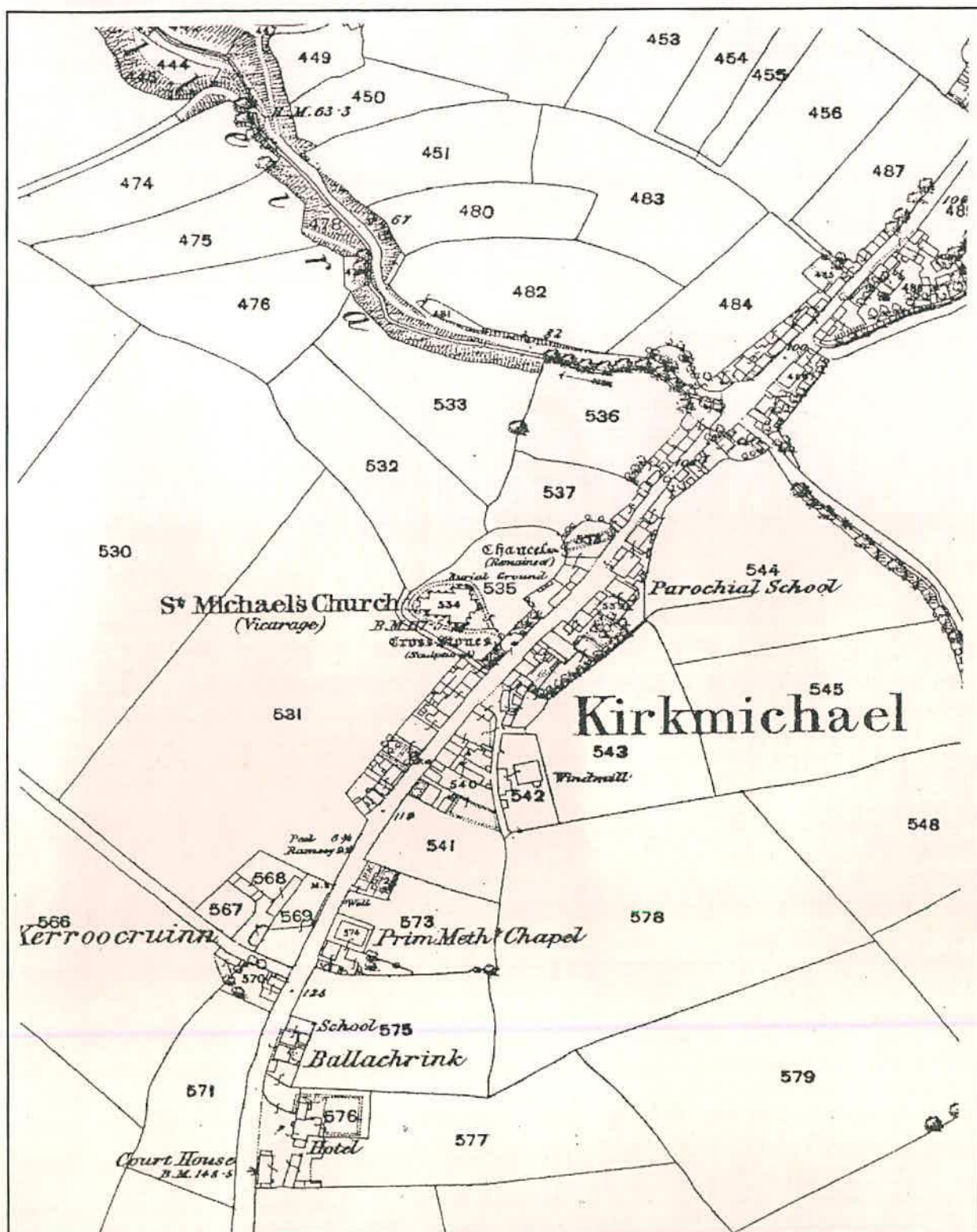
APPENDICES

APPENDIX 1 1869 Ordnance Survey map

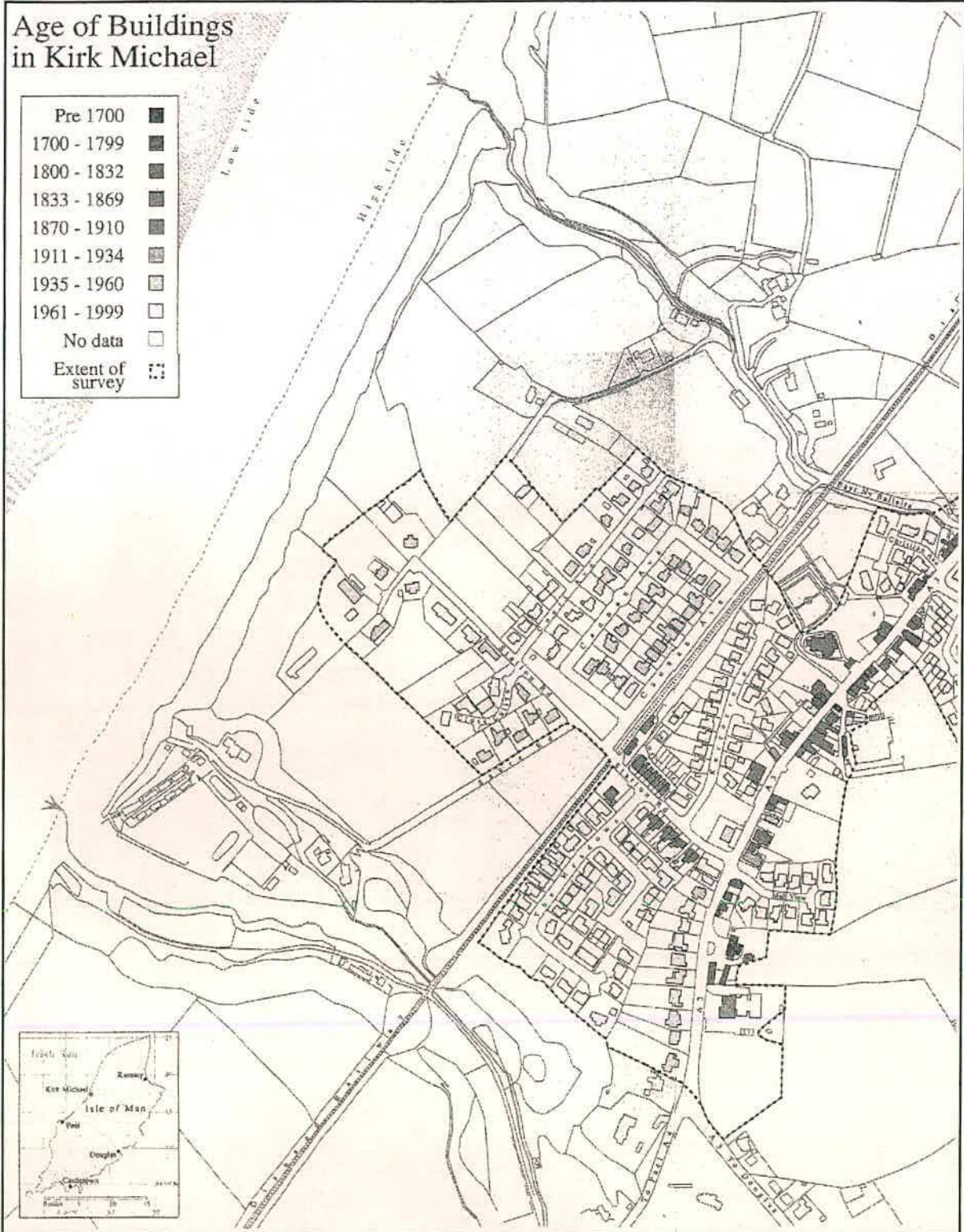
APPENDIX 2 1999 Oxford Brooks Buildings Survey

APPENDIX 3 1967 Ordnance Survey Map Revision

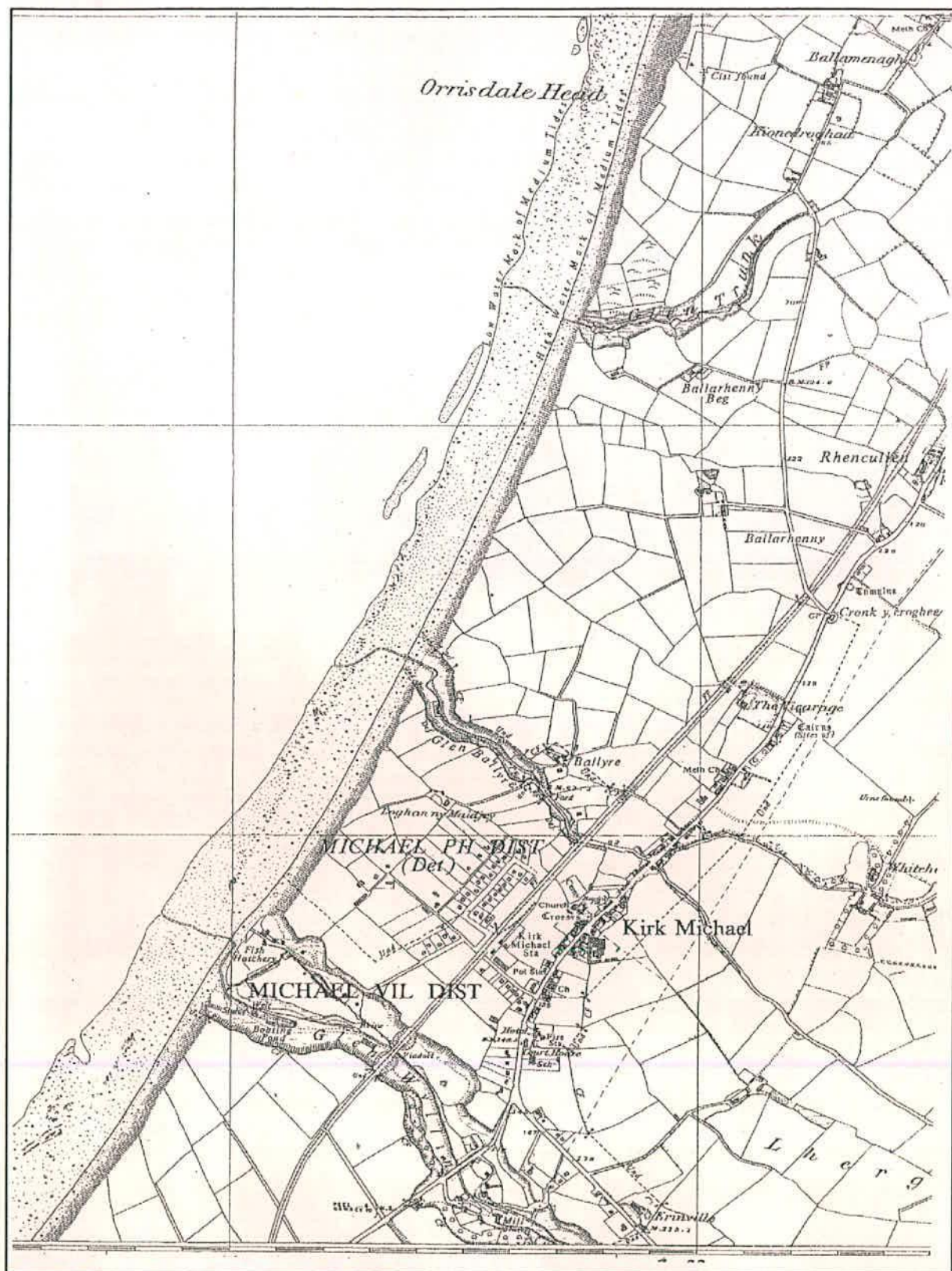
APPENDIX 4 2006 Conservation Area



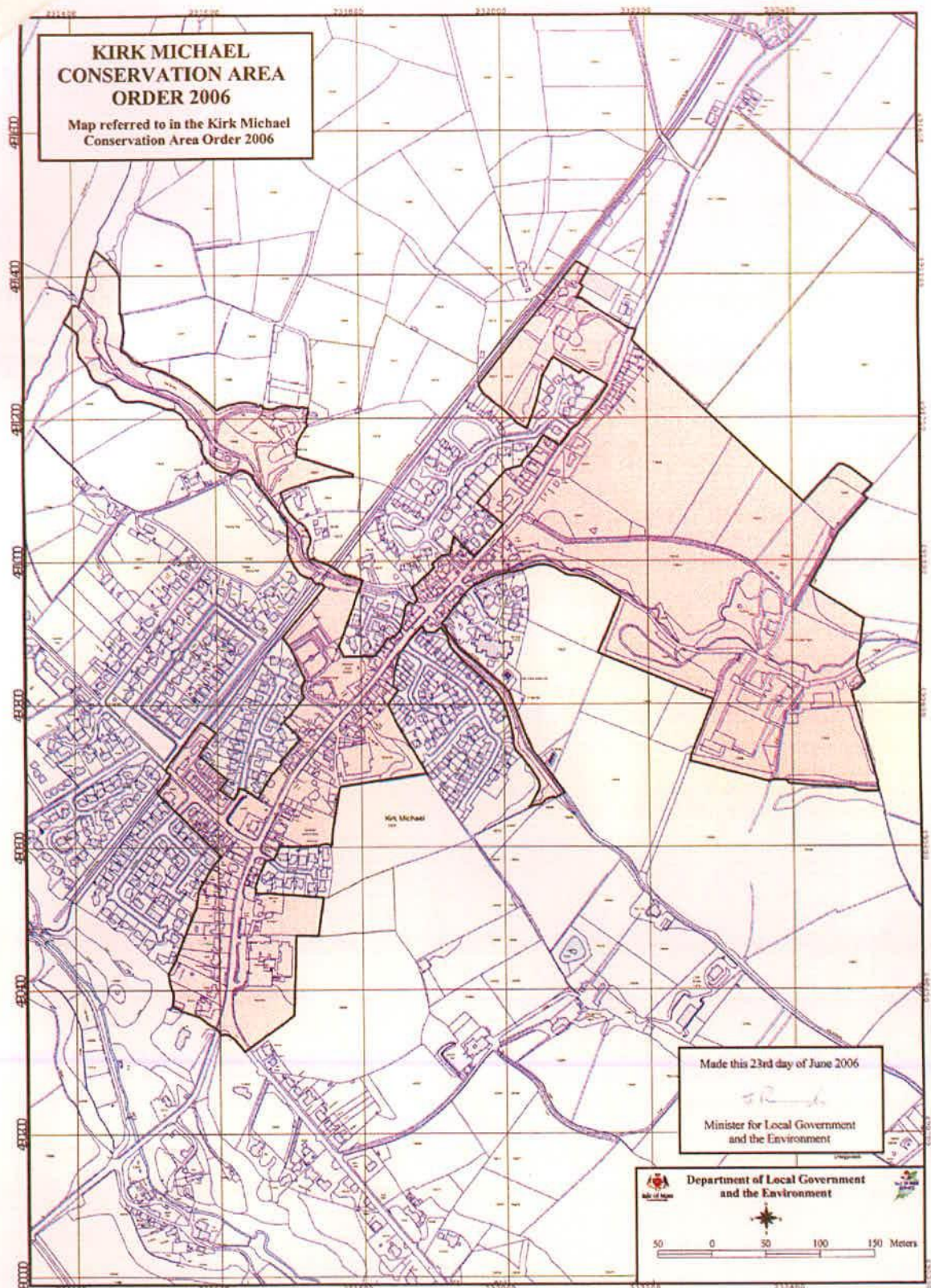
Appendix 1



Appendix 2



Appendix 3



Appendix 4



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